

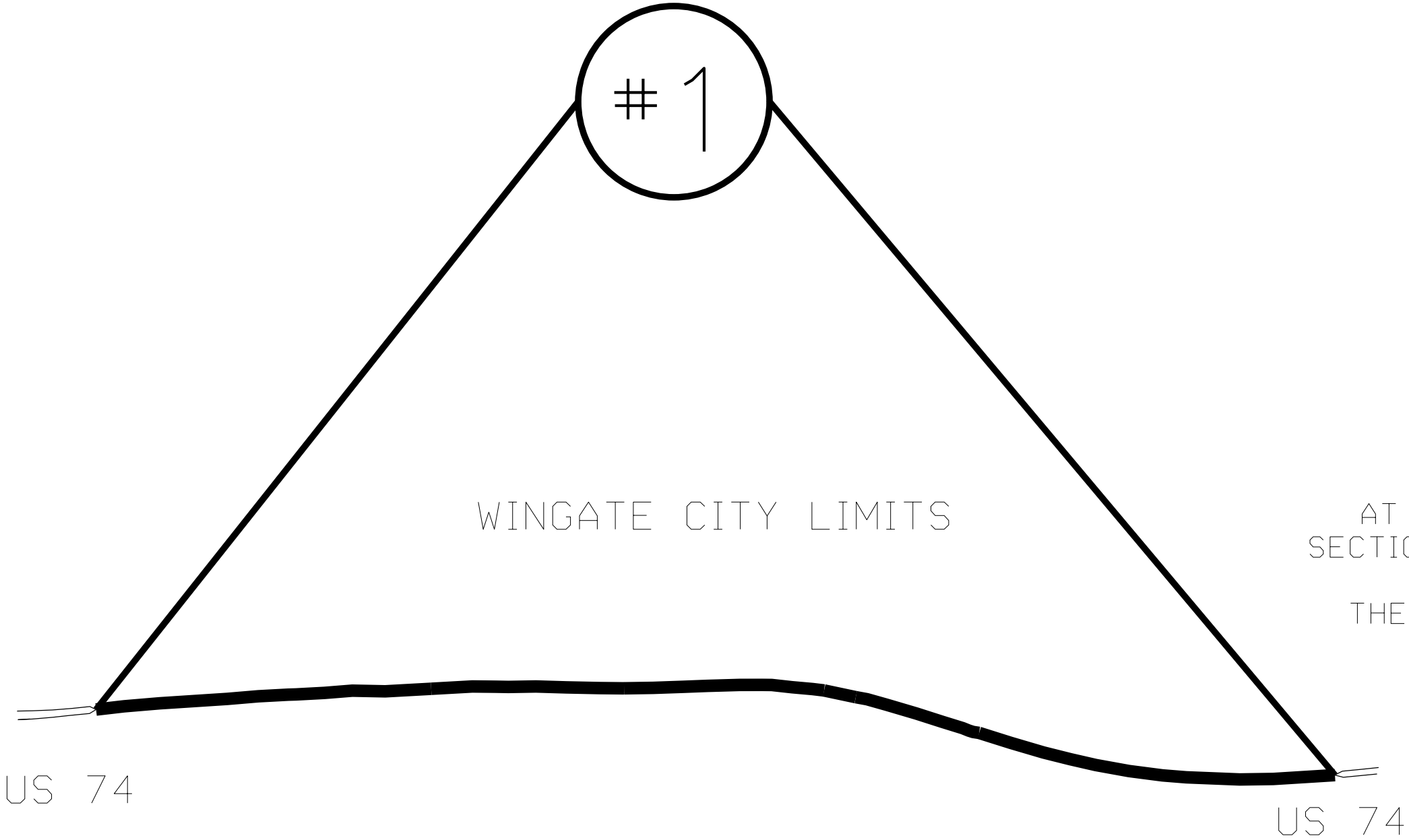
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	1	
F.A. PROJECT NO.			



ENLARGED MUNICIPAL AND SUBURBAN AREAS
UNION COUNTY
NORTH CAROLINA

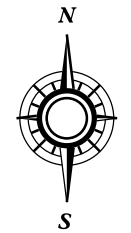
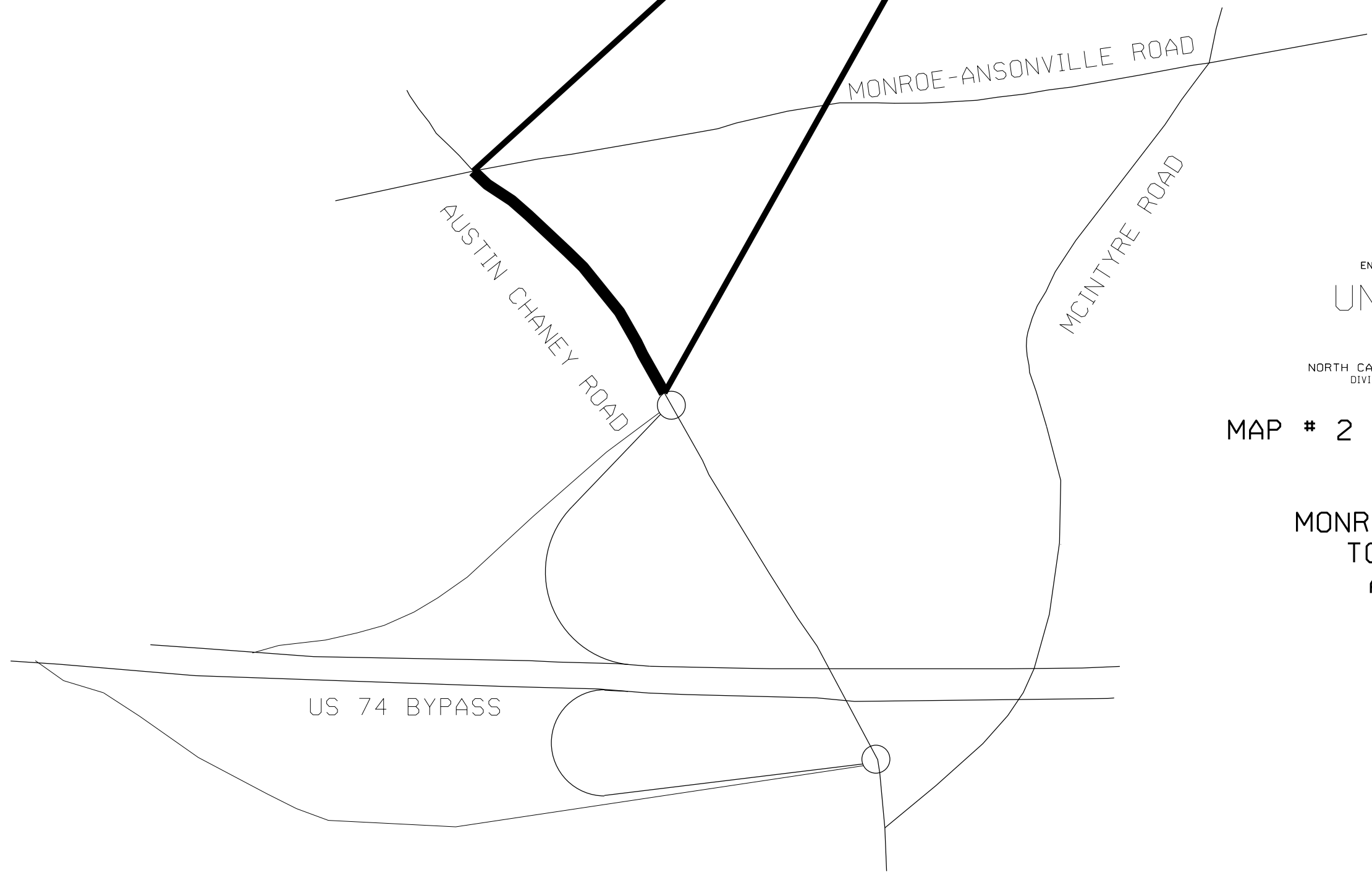
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

Map #1 US 74
1.82 MILES
FROM PAVEMENT JOINT
AT BEGINNING OF THE FIVE LANE
SECTION IN WINGATE TO PAVEMENT JOINT
AT THE END OF
THE FIVE LANE SECTION IN WINGATE



STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
	2022CPT.10.11.10901 2022CPT.10.11.20901	2	
F.A. PROJECT NO.			

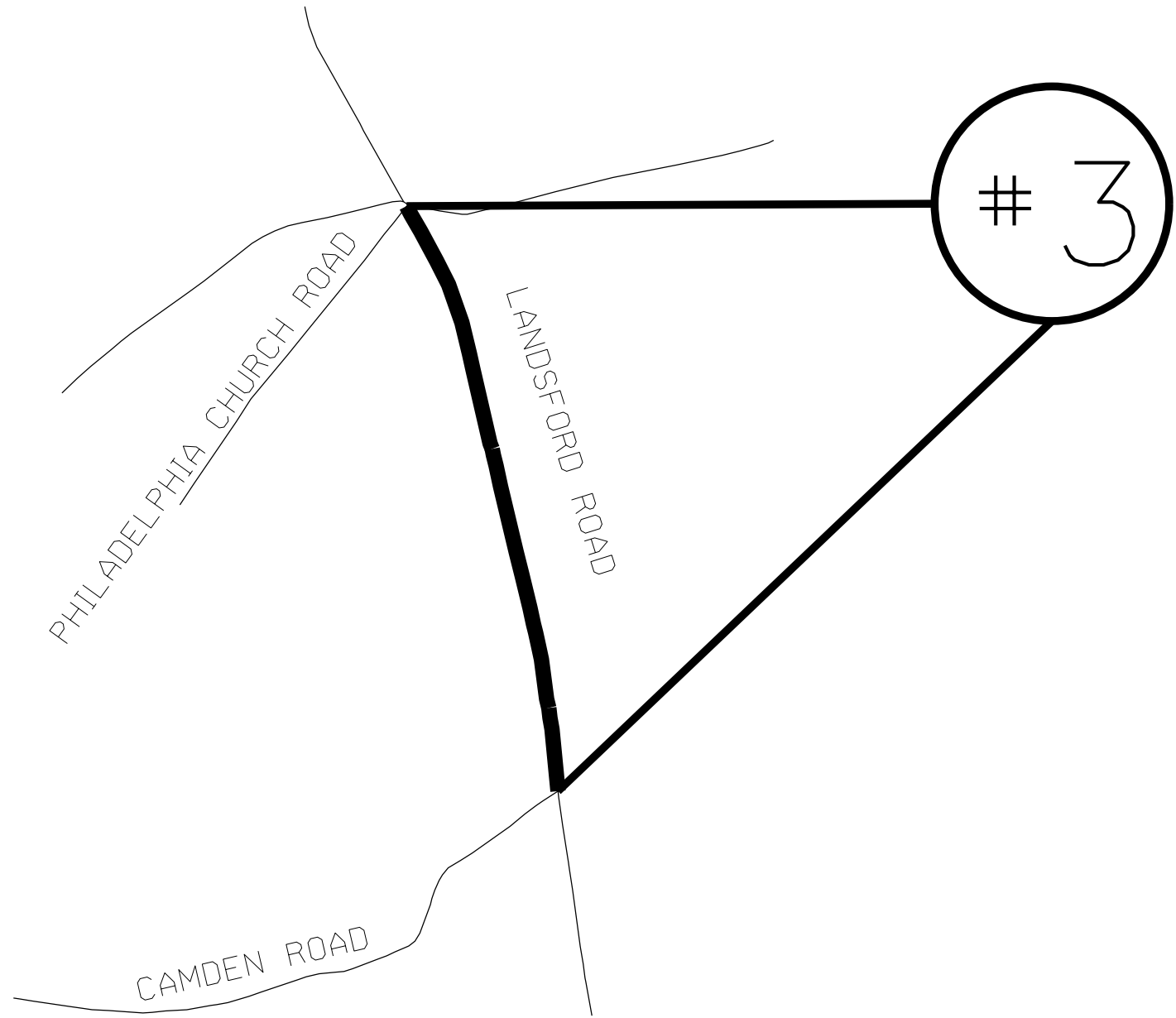
2



ENLARGED MUNICIPAL AND SUBURBAN AREAS
UNION COUNTY
NORTH CAROLINA
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP # 2 SR 1758 AUSTIN CHANEY ROAD
0.29 MILES
FROM SR 1751
MONROE-ANSONVILLE ROAD
TO PAVEMENT JOINT
AT TRAFFIC CIRCLE

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	3	
F.A. PROJECT NO.			



ENLARGED MUNICIPAL AND SUBURBAN AREAS
UNION COUNTY
NORTH CAROLINA
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #3 SR 1005 LANDSFORD ROAD
1.17 MILES
FROM PAVEMENT JOINT AT
SR 1929 PHILADELPHIA CHURCH ROAD
TO SR 1934 CAMDEN ROAD

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
	2022CPT.10.11.10901 2022CPT.10.11.20901	4	
F.A. PROJECT NO.			



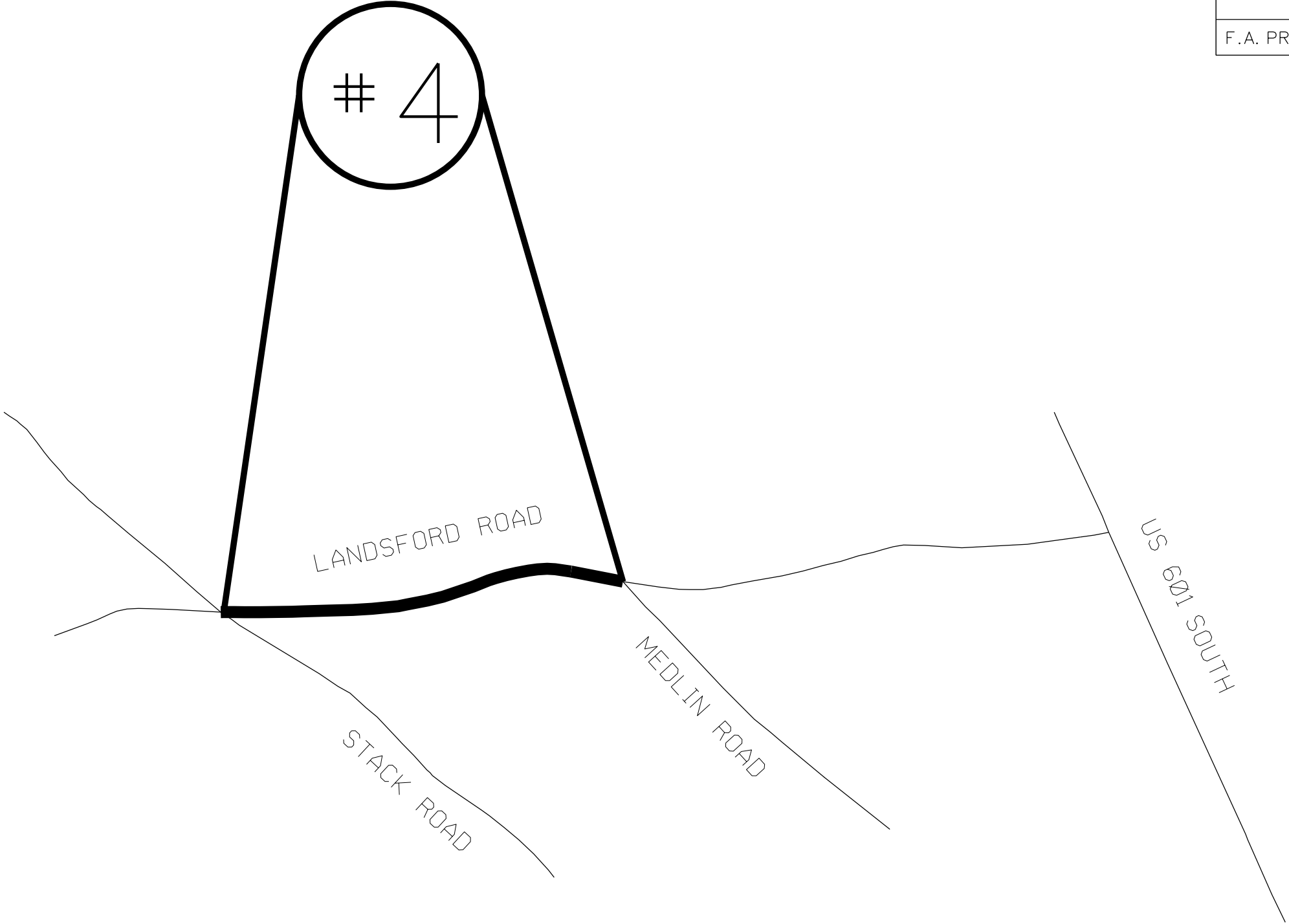
ENLARGED MUNICIPAL AND SUBURBAN AREAS

UNION COUNTY

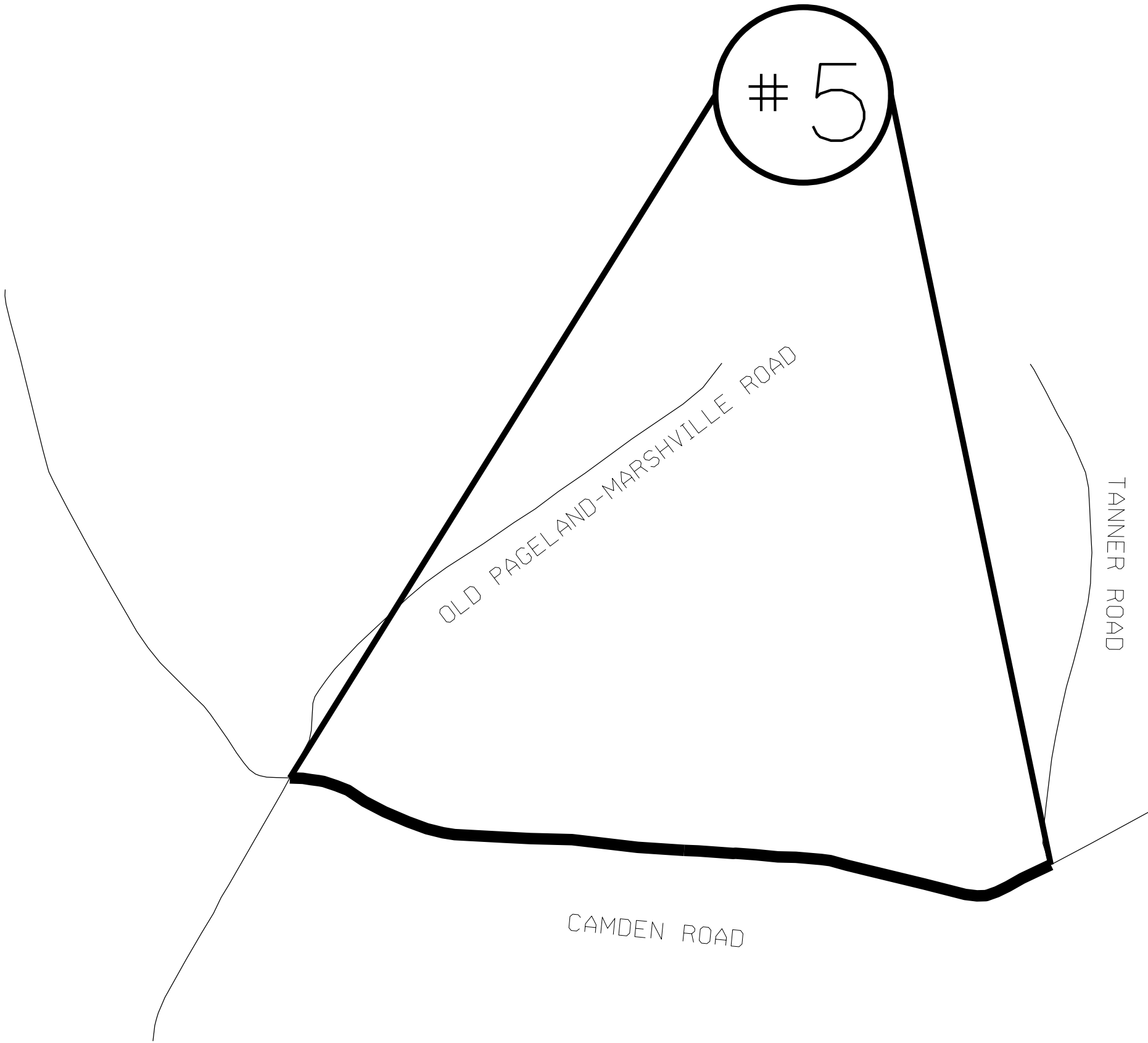
NORTH CAROLINA

PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #4 SR 2254 LANDSFORD ROAD
0.81 MILES
FROM SR 2114 MEDLIN ROAD
TO SR 2115 STACK ROAD



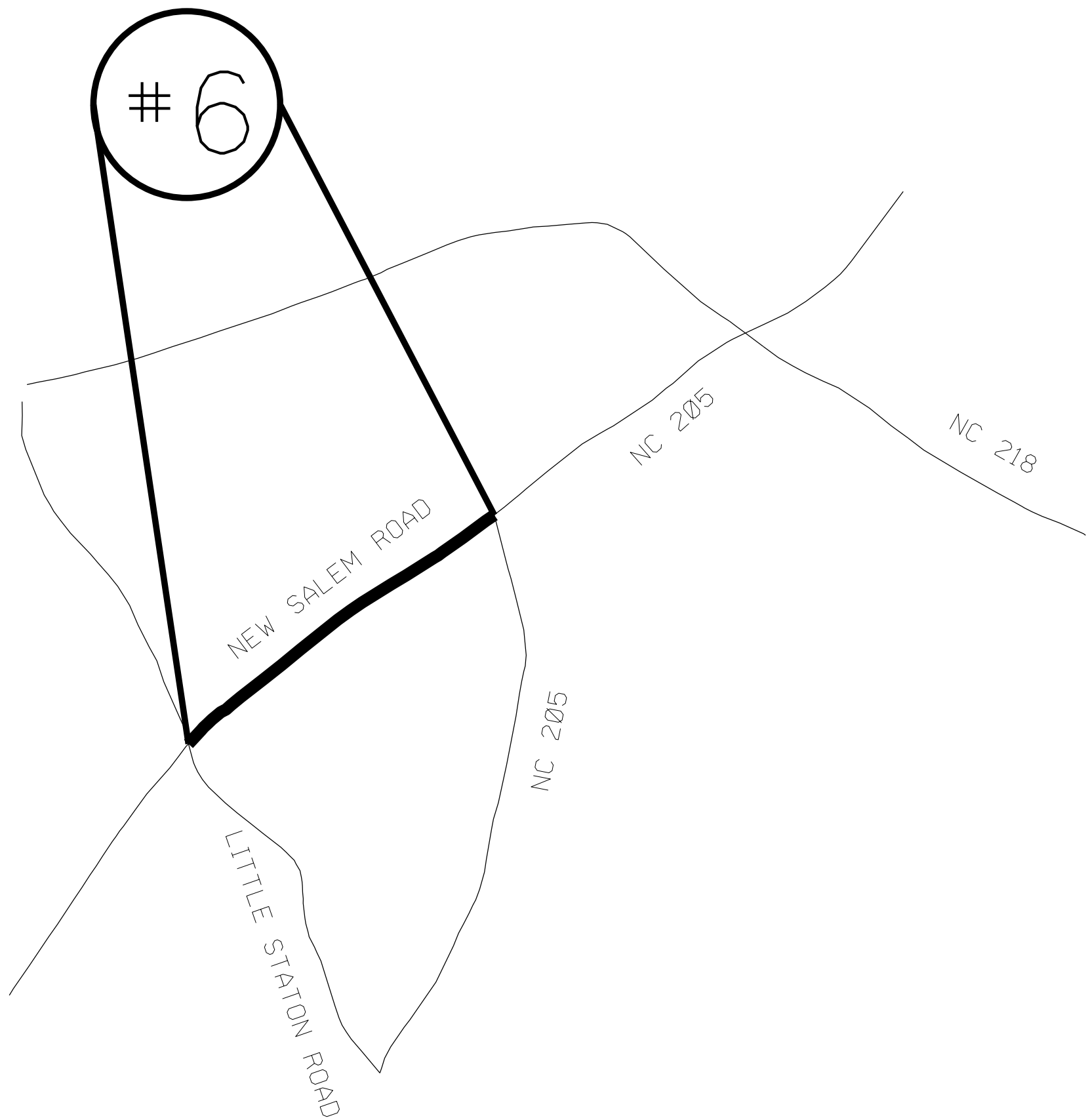
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
	2022CPT.10.11.10901 2022CPT.10.11.20901	5	
F.A. PROJECT NO.			



ENLARGED MUNICIPAL AND SUBURBAN AREAS
UNION COUNTY
NORTH CAROLINA
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #5 SR 1934 CAMDEN ROAD
1.4 MILES
FROM SR 1937
OLD PAGELAND-MARSHVILLE ROAD
TO SR 1935 TANNER ROAD

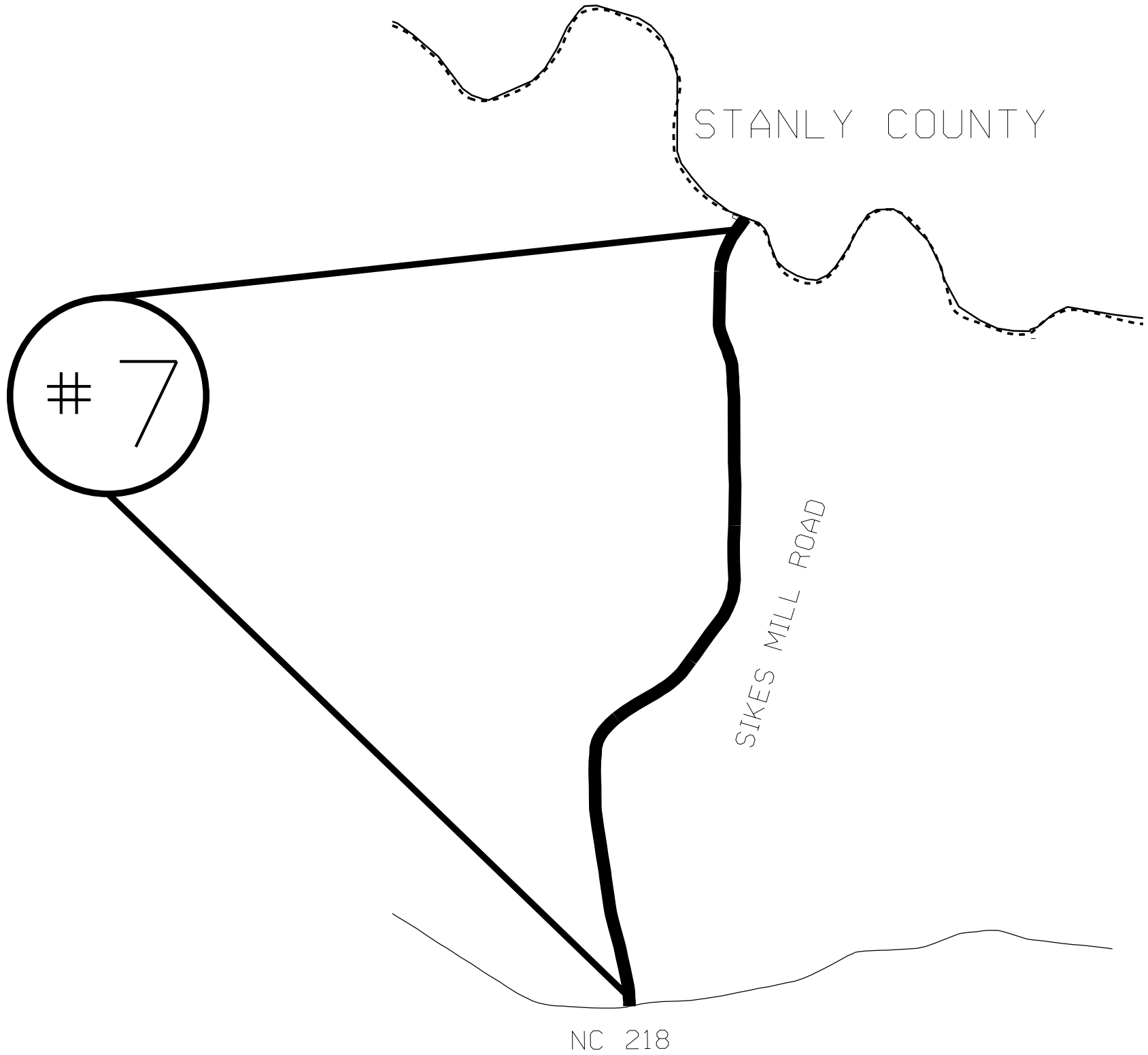
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
	2022CPT.10.11.10901 2022CPT.10.11.20901	6	
F.A. PROJECT NO.			



ENLARGED MUNICIPAL AND SUBURBAN AREAS
UNION COUNTY
NORTH CAROLINA
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #6 SR 1627 NEW SALEM ROAD
0.95 MILES
FROM PAVEMENT JOINT AT NC 205
TO SR 1659 LITTLE STATON ROAD

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
	2022CPT.10.11.10901 2022CPT.10.11.20901	7	
F.A. PROJECT NO.			



ENLARGED MUNICIPAL AND SUBURBAN AREAS

UNION COUNTY

NORTH CAROLINA

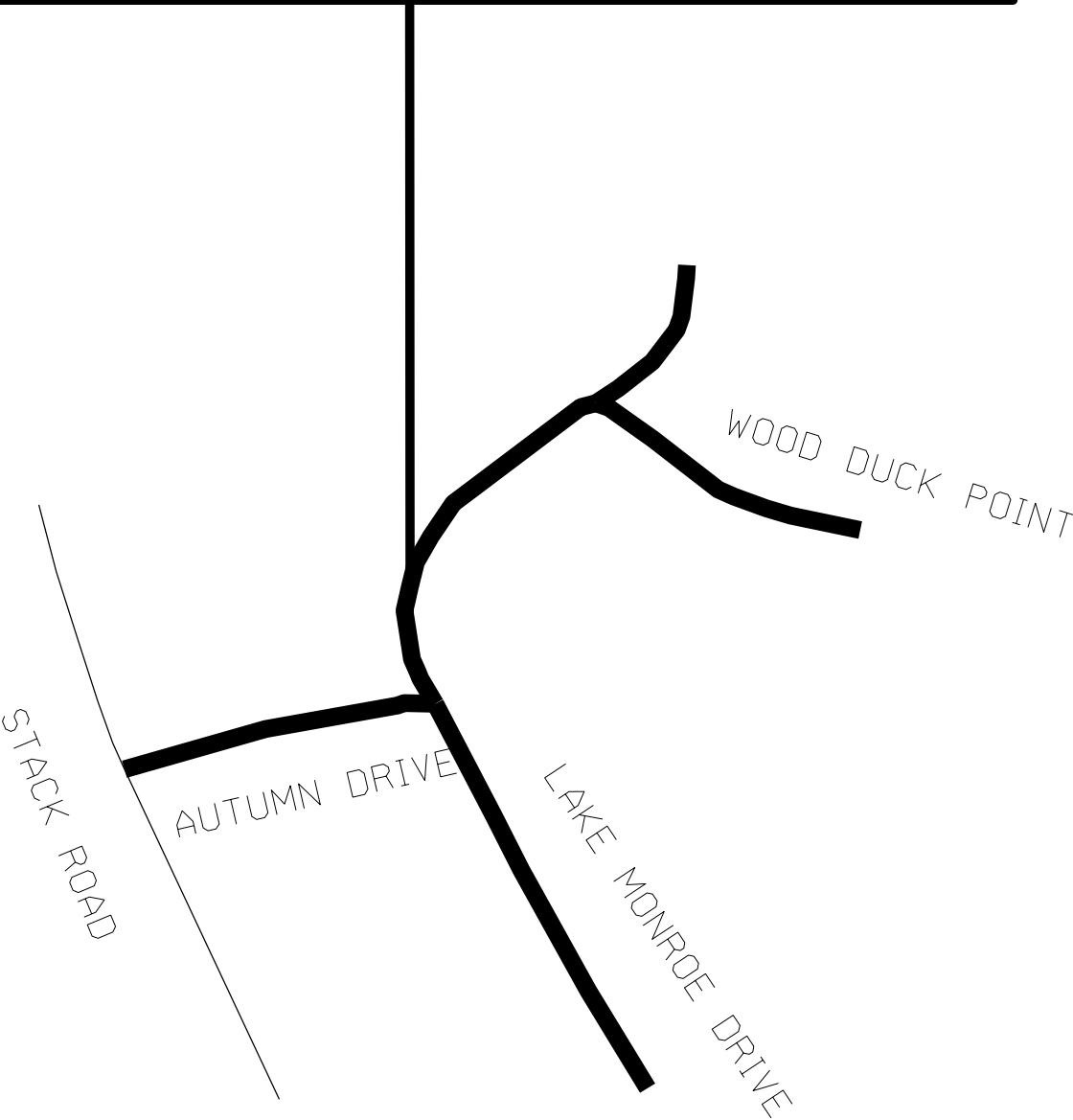
PREPARED BY: EHE

NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #7 SR 1606 SIKES MILL ROAD
2.86 MILES
FROM UNION COUNTY LINE
TO NC 218

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
	2022CPT.10.11.10901 2022CPT.10.11.20901	8	
F.A. PROJECT NO.			

8 - # 10



ENLARGED MUNICIPAL AND SUBURBAN AREAS

UNION COUNTY

NORTH CAROLINA

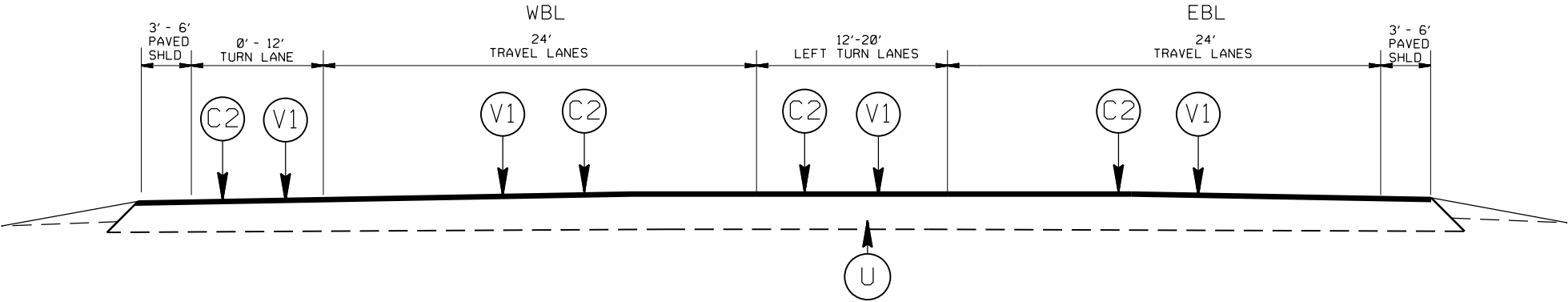
PREPARED BY THE
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #8 SR 2288 AUTMN DRIVE
Ø.13 MILES
FROM SR 2115 STACK ROAD TO
SR 2219 LAKE MONROE DRIVE

MAP #9 SR 2219 LAKE MONROE DRIVE
Ø.47 MILES
FROM END OF MAINTANANCE
TO END OF MAINTANANCE

MAP #10 SR 2220 WOOD DUCK POINT
Ø.13 MILES
FROM SR 2219 LAKE MONROE DRIVE
TO END OF MAINTANANCE

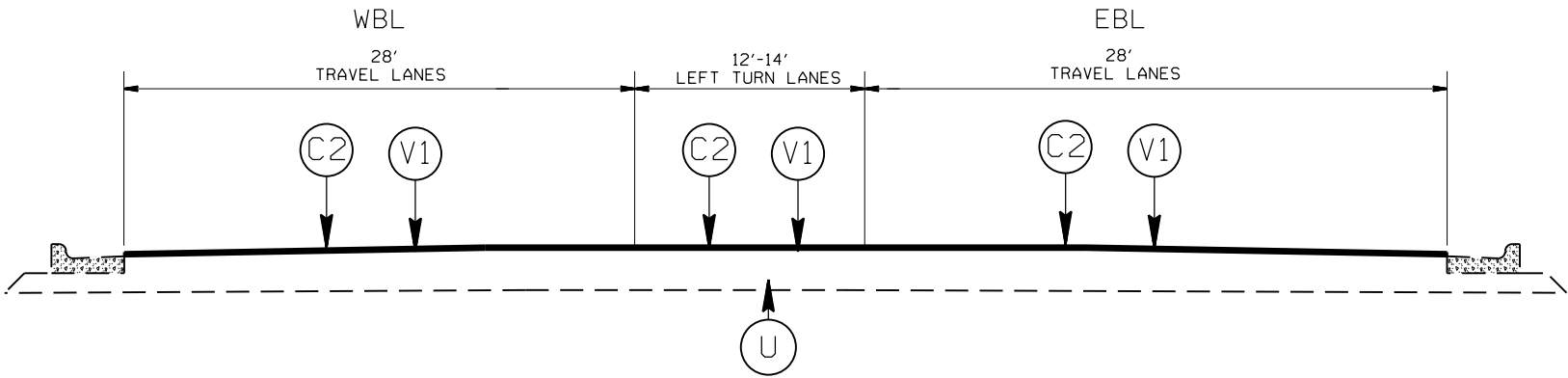
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	9	
F.A. PROJECT NO.			




TYPICAL SECTION NO. 1
US 74 (MAP 1)
APPROX. STA: 10+00 TO 16+80
APPROX. STA: 101+45 TO 106+30

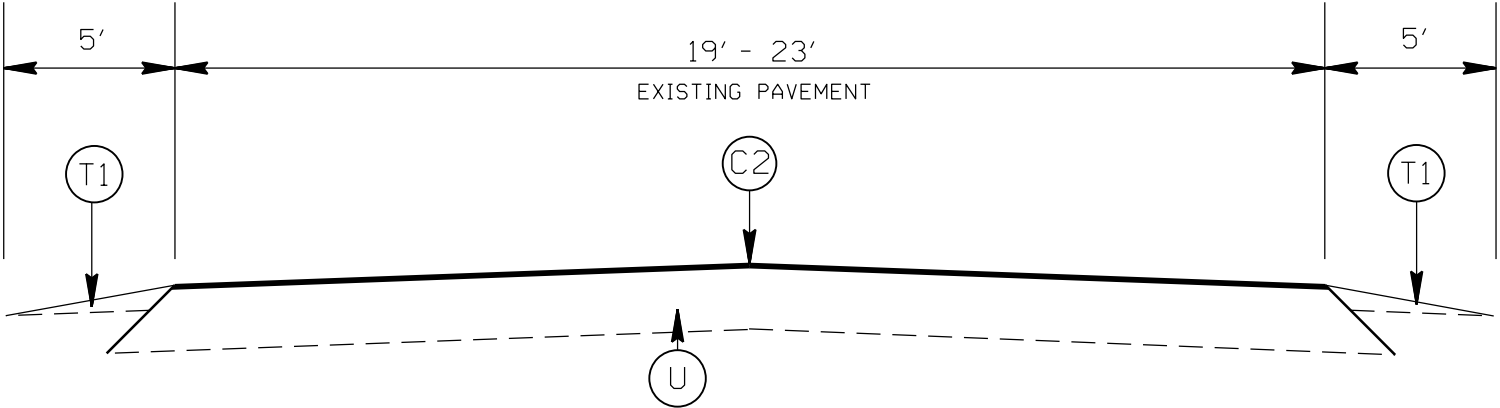
PAVEMENT SCHEDULE

C1	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C4	PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD.
C5	PROP. ASPHALT SURFACE TREATMENT, #67 MAT COAT
C6	PROP. ASPHALT SURFACE TREATMENT, #78M MAT COAT
D1	PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T1	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING PAVEMENT, 1.5" DEPTH.

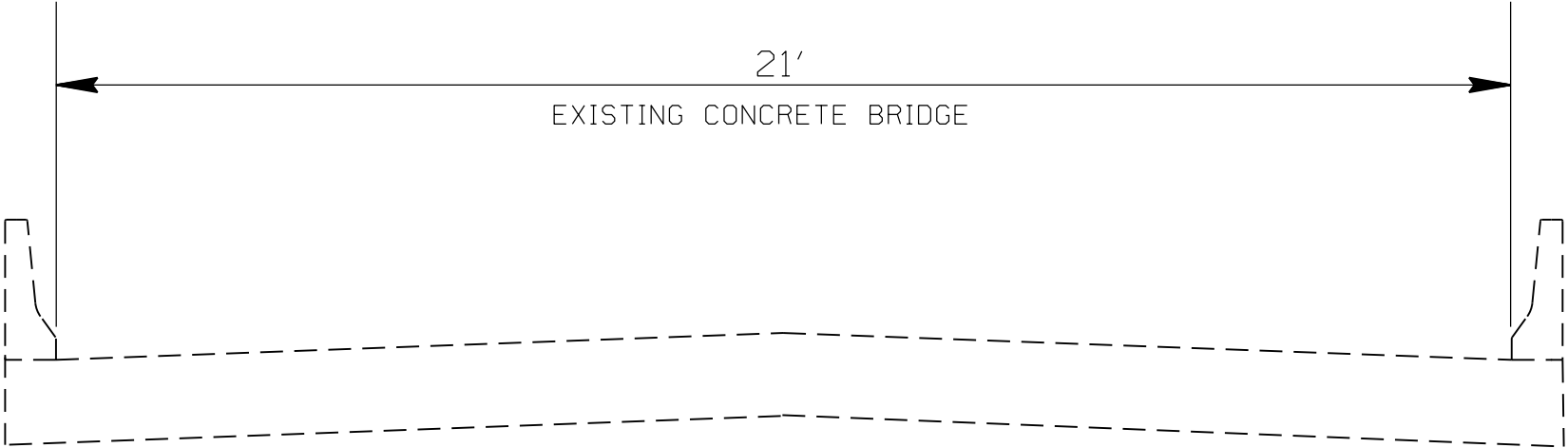


TYPICAL SECTION NO. 2
US 74 (MAP 1)
APPROX. STA: 16+80 TO 101+45

2022-2023 UNION COUNTY RESURFACING			REVISIONS	
SCALE	-NA-			
DATE	9/21			
DWG. BY	AMO			
DESIGN BY	AMO			
APPROVED				



TYPICAL SECTION NO. 3
SR 1758 AUSTIN CHANEY ROAD (MAP 2)
SR 1627 NEW SALEM ROAD (MAP 6)
SR 1606 SIKES MILL ROAD (MAP 7)




TYPICAL SECTION 4
NO WORK ON CONCRETE BRIDGE
SR 1606 SIKES MILL ROAD (MAP 7)

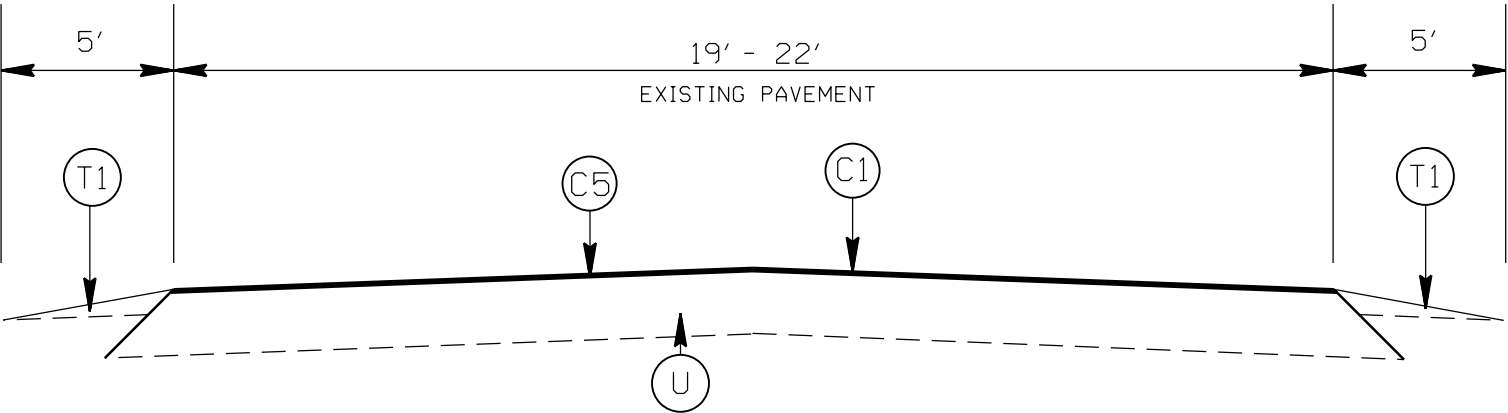
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	10	
F.A. PROJECT NO.			

PAVEMENT SCHEDULE

C1	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C4	PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD.
C5	PROP. ASPHALT SURFACE TREATMENT, #67 MAT COAT
C6	PROP. ASPHALT SURFACE TREATMENT, #78M MAT COAT
D1	PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T1	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.

2022-2023 UNION COUNTY RESURFACING			
SCALE	-NA-		REVISIONS
DATE	9/21		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED			

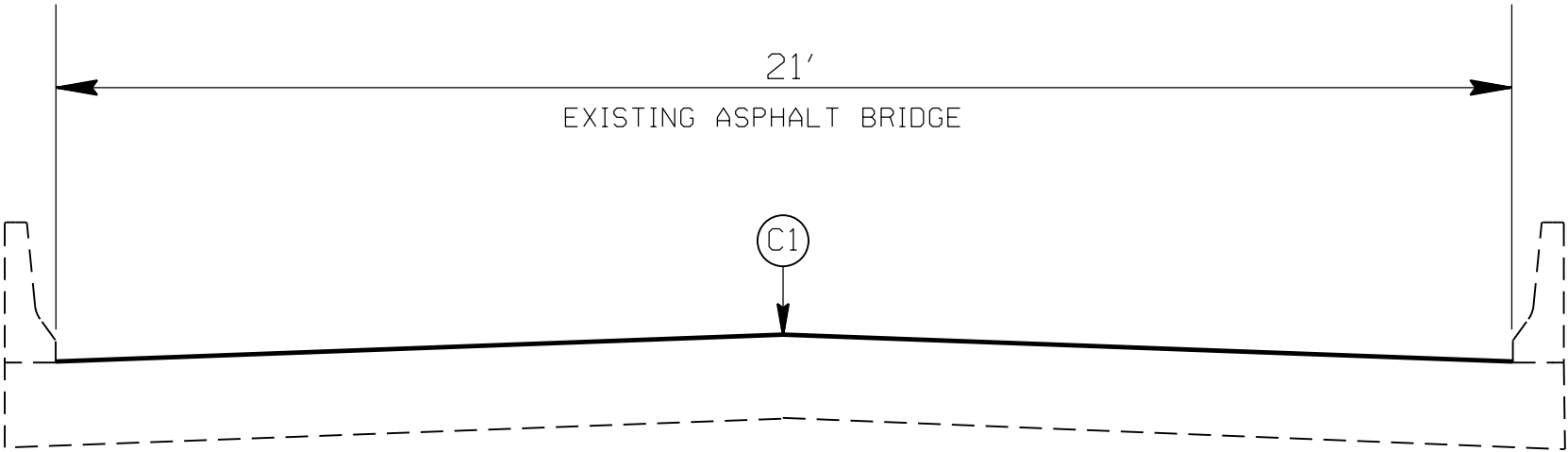
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	11	
F.A. PROJECT NO.			




TYPICAL SECTION NO. 5
SR 1005 LANDSFORD ROAD (MAP 3)

PAVEMENT SCHEDULE

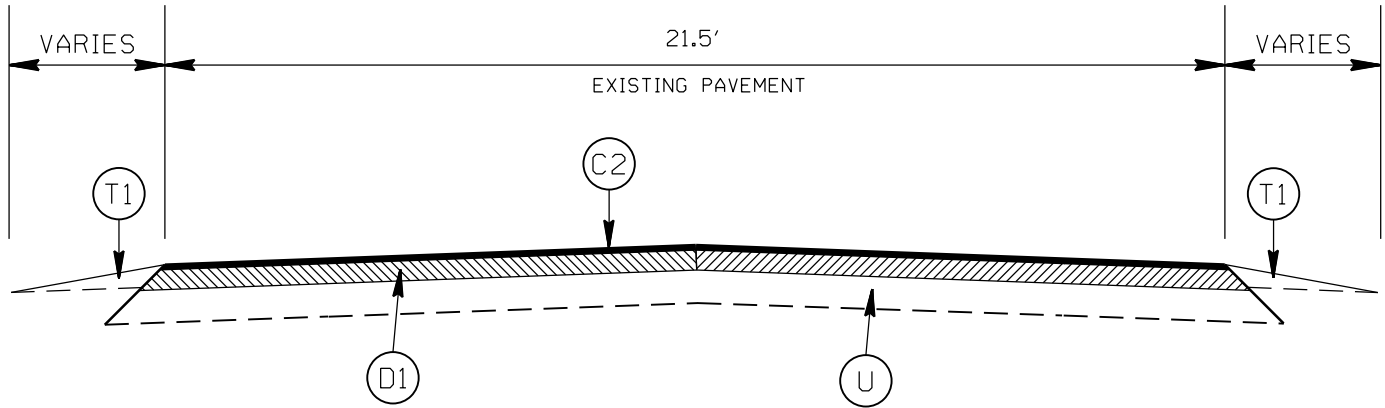
C1	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C4	PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD.
C5	PROP. ASPHALT SURFACE TREATMENT, #67 MAT COAT
C6	PROP. ASPHALT SURFACE TREATMENT, #78M MAT COAT
D1	PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T1	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.



TYPICAL SECTION 6
SR 1005 LANDSFORD ROAD (MAP 3)

2022-2023 UNION COUNTY RESURFACING			
SCALE	-1A-		REVISIONS
DATE	9/21		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED			

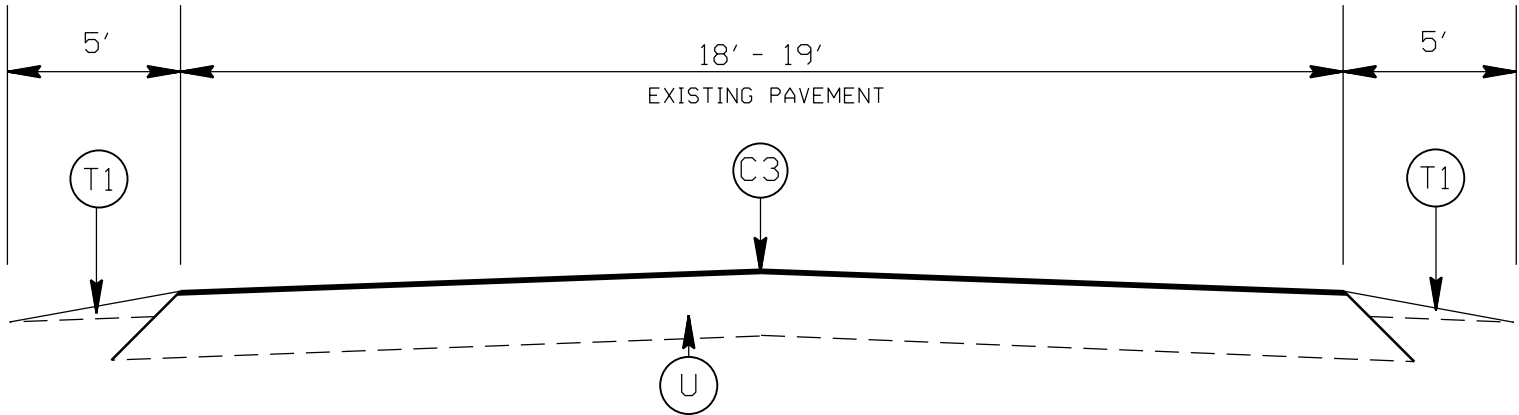
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	12	
F.A. PROJECT NO.			




TYPICAL SECTION NO. 7
SR 2254 LANDSFORD ROAD (MAP 4)

PAVEMENT SCHEDULE

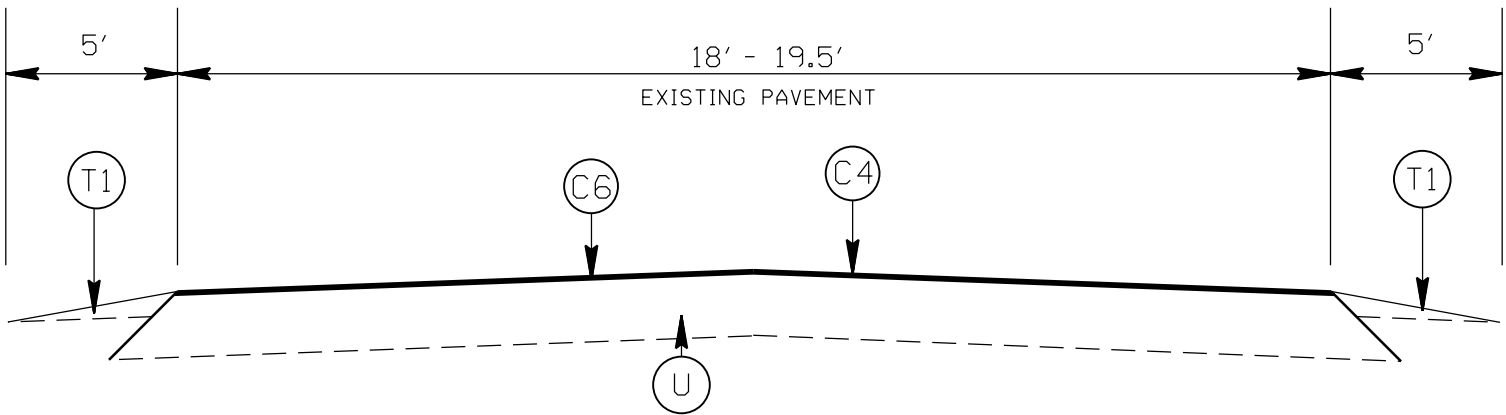
C1	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C4	PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD.
C5	PROP. ASPHALT SURFACE TREATMENT, #67 MAT COAT
C6	PROP. ASPHALT SURFACE TREATMENT, #78M MAT COAT
D1	PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T1	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.



TYPICAL SECTION NO. 8
SR 1934 CAMDEN ROAD (MAP 5)

2022-2023 UNION COUNTY RESURFACING			
SCALE	NA		REVISIONS
DATE	9/21		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED			


STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	13	
F.A. PROJECT NO.			



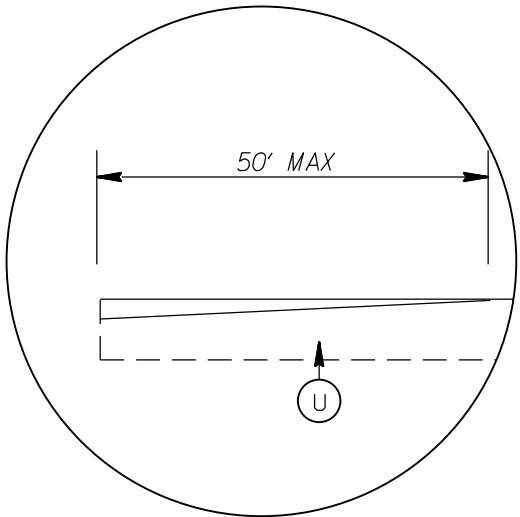
TYPICAL SECTION NO. 9
SR 2288 AUTUMN DRIVE (MAP 8)
SR 2219 LAKE MONROE DRIVE (MAP 9)
SR 2220 WOOD DUCK POINT (MAP 10)

PAVEMENT SCHEDULE

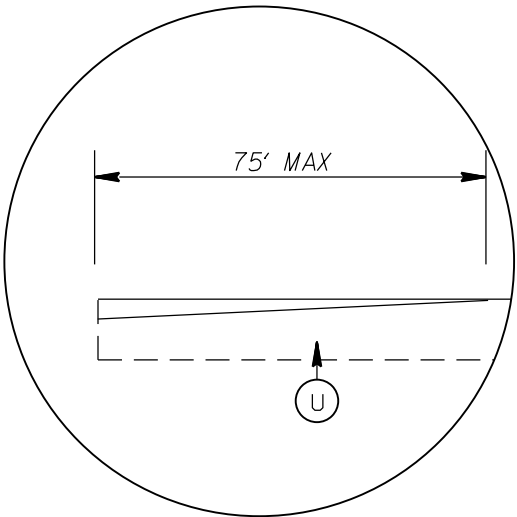
C1	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C4	PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD.
C5	PROP. ASPHALT SURFACE TREATMENT, #67 MAT COAT
C6	PROP. ASPHALT SURFACE TREATMENT, #78M MAT COAT
D1	PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T1	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.

2022-2023 UNION COUNTY RESURFACING			
SCALE	-NA-		REVISIONS
DATE	9/21		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED			

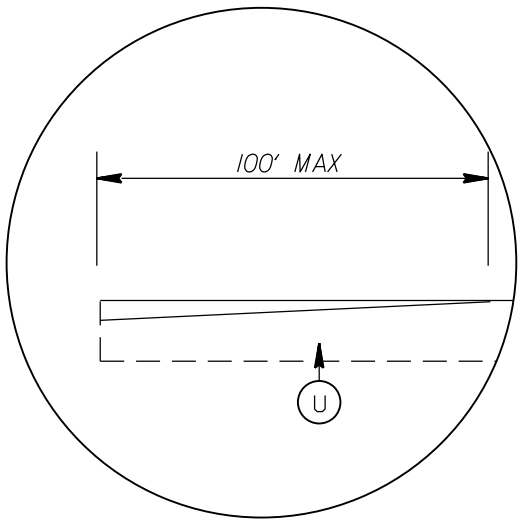
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	14	
F.A. PROJECT NO.			



DETAIL FOR INCIDENTAL MILLING (0" TO 1.0")
TIE IN

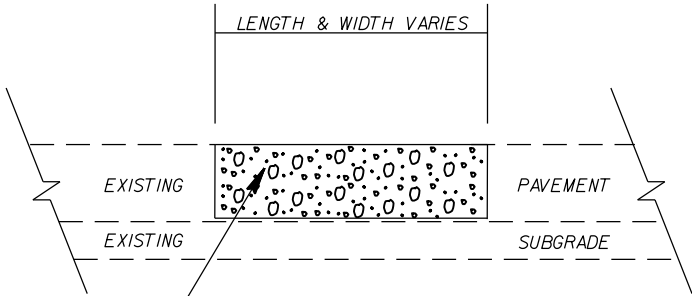


DETAIL FOR INCIDENTAL MILLING (0" TO 1.5")
TIE IN



DETAIL FOR INCIDENTAL MILLING (0" TO 2.0")
TIE IN

PATCHING DETAIL



RATE IS VARIABLE AND SHALL BE
AS DIRECTED BY THE ENGINEER.
ASPHALT TYPE 119.0C SHALL BE PLACED.

PAVEMENT SCHEDULE

C1	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C4	PROP. APPROX. 1.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD.
C5	PROP. ASPHALT SURFACE TREATMENT, #67 MAT COAT
C6	PROP. ASPHALT SURFACE TREATMENT, #78M MAT COAT
D1	PROPOSED 2.5" ASPHALT CONC. INTERMEDIATE COURSE, TYPE 119.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T1	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.


2022-2023
UNION COUNTY RESURFACING

SCALE	-NA-		REVISIONS	
DATE	9/21			
DWG. BY	AMO			
DESIGN BY	AMO			
APPROVED				

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	15	
F.A. PROJECT NO.			

NOTES:

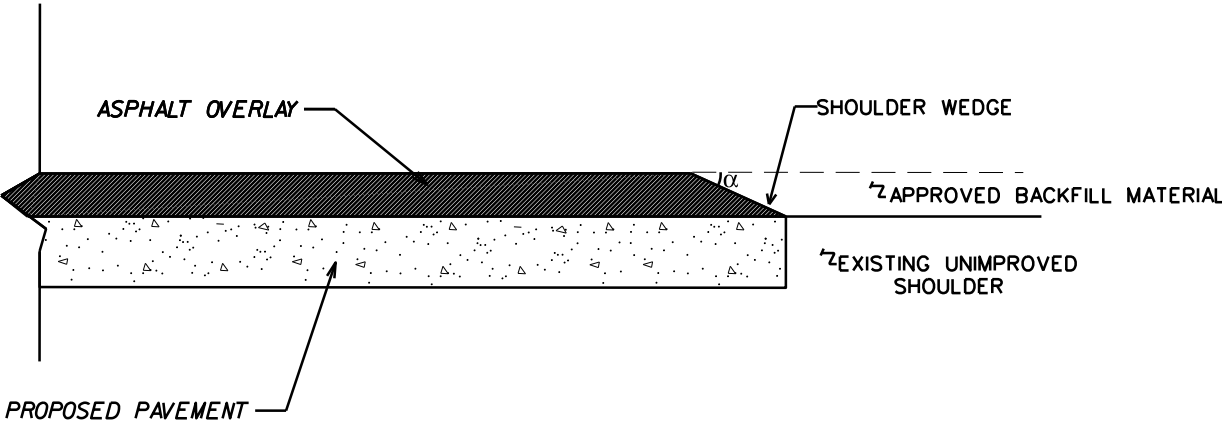
- 1: ON MAP 3, MILL AND FILL ASPHALT BRIDGE WITH 2.0" S9.5C. DO NOT PLACE MAT COAT ON BRIDGE.
- 2: ON MAP 7, DO NOT PLACE ASPHALT ON CONCRETE BRIDGE.
- 3: SHOULDER RECONSTRUCTION WILL BE AS DIRECTED BY THE ENGINEER.
- 4: PATCHING ON MAPS 2 AND 5 WILL BE DONE BY STATE FORCES.

2022-2023 UNION COUNTY RESURFACING			REVISIONS	
SCALE	-NA-			
DATE	9/21			
DWG. BY	AMO			
DESIGN BY	AMO			
APPROVED				

NOTES:

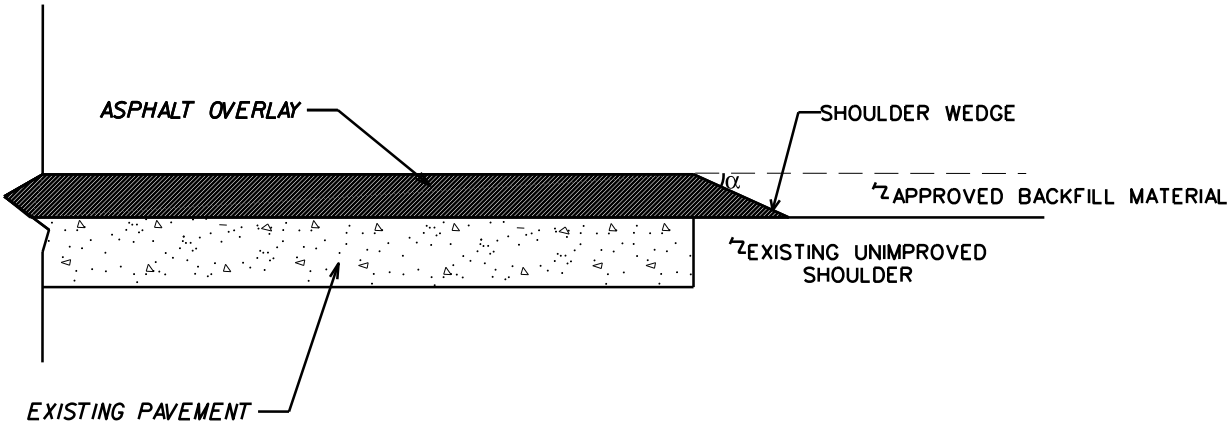
- 1) DETAIL DOES NOT APPLY TO OGAF C AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	16	
F.A. PROJECT NO.			



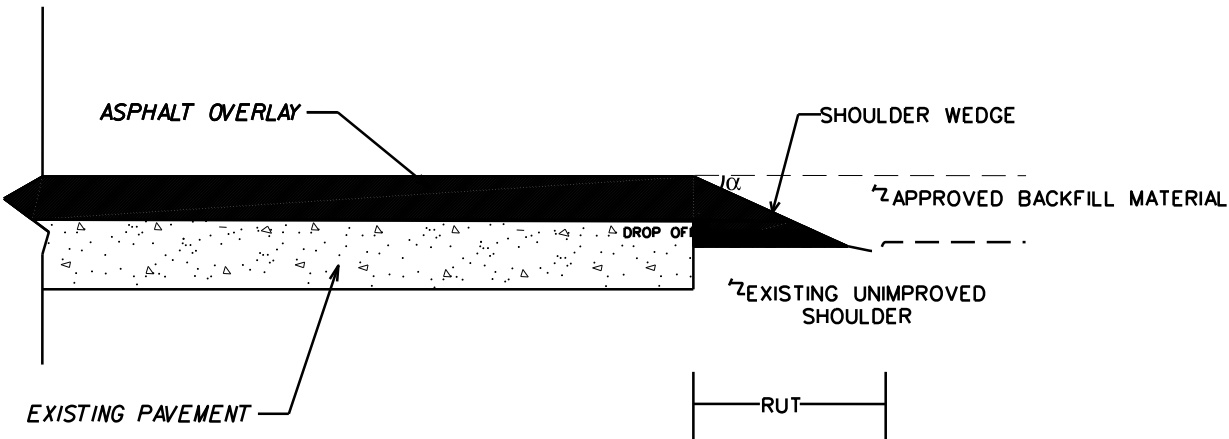
SHOULDER WEDGE DETAIL

(RESURFACING PROJECTS W/ WIDENING OR WITH EXISTING PAVED SHOILDER HAVING NO DROPOFFS)



SHOULDER WEDGE DETAIL


(RESURFACING PROJECTS W/ NO WIDENING)



SHOULDER WEDGE DETAIL

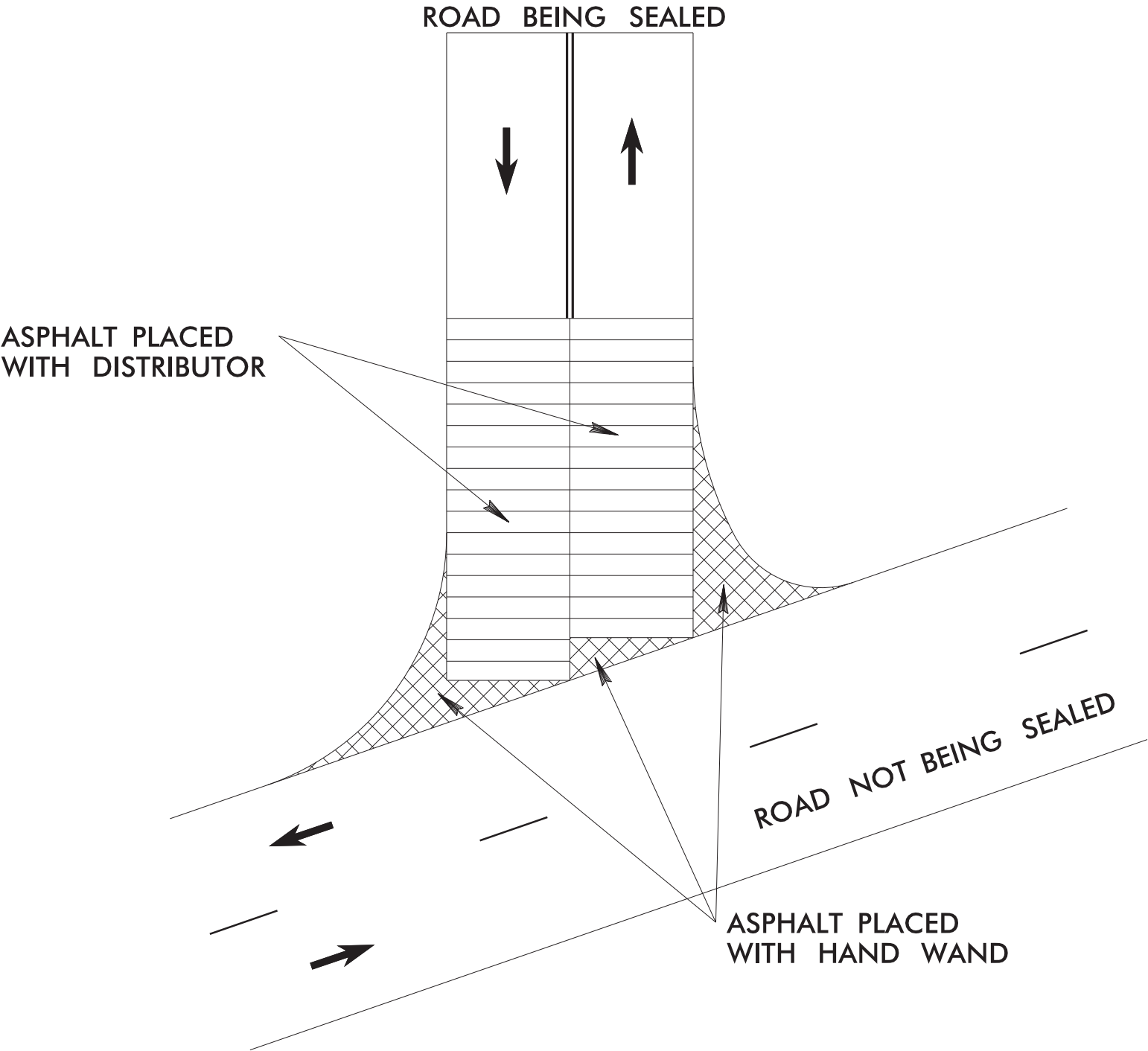
(RESURFACING ADJACENT TO RUTTED SHOULDER)

α = 30 DEGREES

SHOULDER WEDGE DETAILS			
SCALE	-NA-		REVISIONS
DATE	11/20		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED			


INTERSECTION EMULSION PLACEMENT
TWO LANE TWO WAY ROADWAY

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	17	
F.A. PROJECT NO.			




LEGEND


- ➔ DIRECTION OF TRAFFIC FLOW
- SKIP LINES
- == DOUBLE YELLOW LINES


INTERSECTION EMULSION PLACEMENT			
SCALE	N/A		REVISIONS
DATE	9-2019		
DWG. BY	TBL		
DESIGN BY	TBL		
APPROVED	TWB		

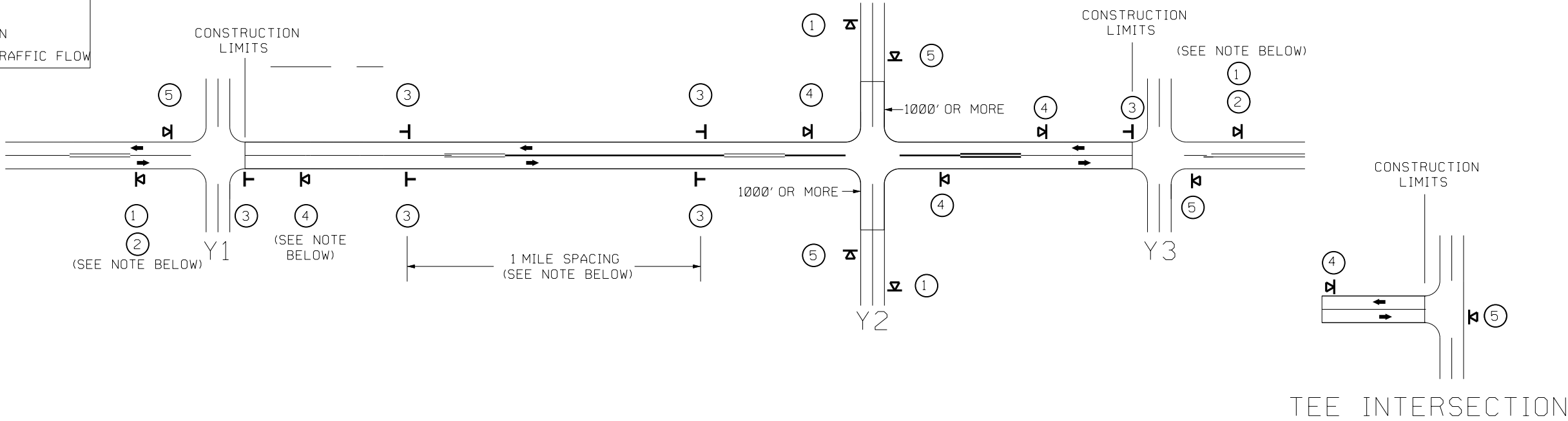
SIGNING FOR ASPHALT SURFACE TREATMENT

LEGEND

 PORTABLE SIGN








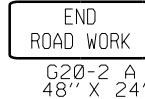
 STATIONARY SIGN

 DIRECTION OF TRAFFIC FLOW




MAINLINE (-L-) SIGNING


-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	<div><div>1</div><div><div>W20-1 48" X 48"</div></div></div> <div><div>2</div><div><div>W7-3aP 24" X 18"</div></div></div>	<div>- PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</div> <div>- SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. 2) SUBDIVISION ROADS ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS.</div>	<div>STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:</div> <div>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS</div>
	<div><div>3</div><div><div>W8-7 48" X 48"</div></div><div><div>SP 48" X 48"</div></div></div>	<div>- ALTERNATE THE FOLLOWING TWO SIGNS:</div> <div>- STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT".</div> <div>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER.</div> <div>- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</div>	<div>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALLE BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</div> <div><div><div>W20-1 48" X 48"</div></div><div>PLACED 500' IN ADVANCE OF FLAGGER.</div></div> <div><div><div>W20-7 A 48" X 48"</div></div><div>PLACED 250' IN ADVANCE OF FLAGGER.</div></div>
	<div><div>4</div><div><div>SP 13106 48" X 48"</div></div></div>	<div>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</div> <div>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</div> <div>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</div> <div>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</div>	
	<div><div>5</div><div><div>G20-2 A 48" X 24"</div></div></div>	<div>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</div>	
	<div>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</div>		
PS LESS 2 MILES	<div>FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.</div>		

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
WORK ZONE TRAFFIC CONTROL

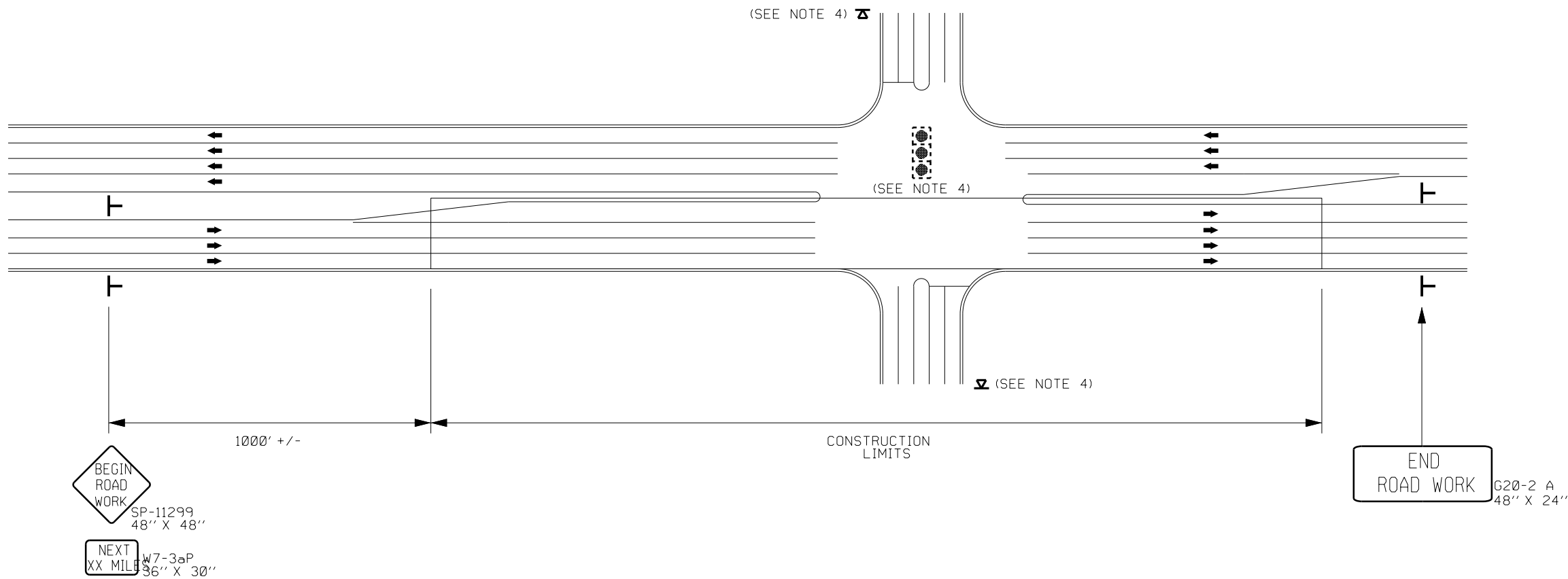


A
ASF



ADVANCE WARNING SIGNS
FOR
2-LANE ROADWAY
ASPHALT SURFACE TREATMEN

URBAN / SUBURBAN WORKZONES



NOTES:

- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW



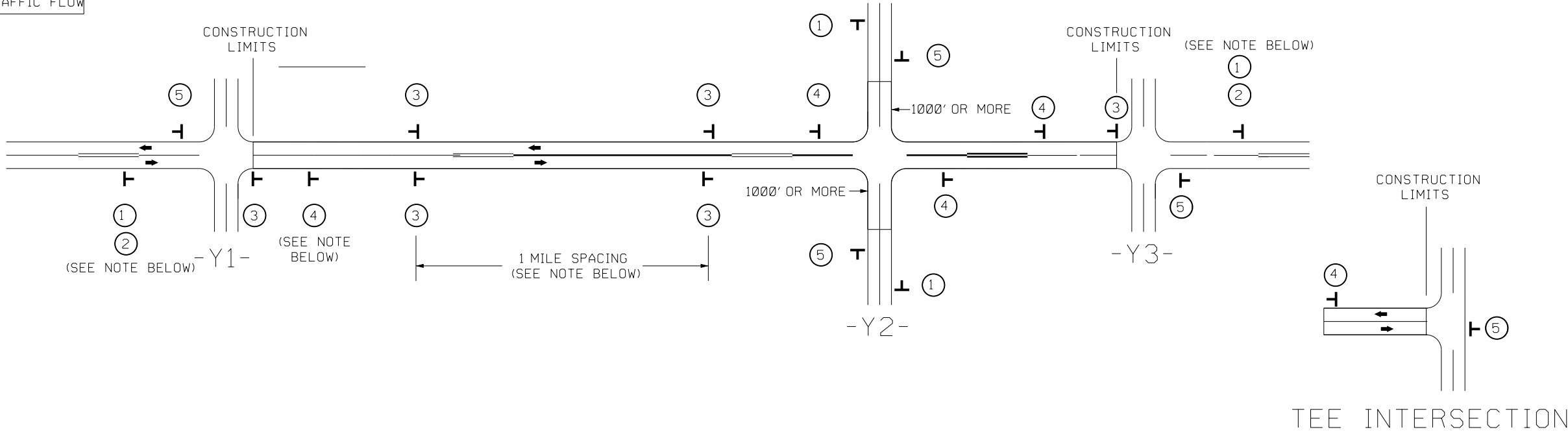
RESURFACING ADVANCE
WARNING SIGNS FOR
URBAN / SUBURBAN
FACILITIES

SIGNING FOR RESURFACING PROJECTS

LEGEND





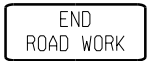
STATIONARY SIGN


DIRECTION OF TRAFFIC FLOW




MAINLINE (-L-) SIGNING

-Y- LINE SIGNING


SIGNING NOTES AND PLACEMENT PER DIRECTION	<div>①</div> <div> W20-1 48" X 48"</div>	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS: 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.
	<div>②</div> <div> W7-3aP 24" X 18"</div>	#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	<div>③</div> <div> SP 13107 48" X 48"</div>	- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY ½ MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	<div>④</div> <div> SP 13106 48" X 48"</div>	- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	<div>⑤</div> <div> G20-2 A 48" X 24"</div>	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	
	THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.		
LESS 2 MILES	FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.		


W20-1
48" X 48"

PLACED 500' IN ADVANCE
OF FLAGGER.


W20-7 A
48" X 48"

PLACED 250' IN ADVANCE
OF FLAGGER.





ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2-LANE ROADWAY
RESURFACING

WATTLE WITH POLYACRYLAMIDE DETAIL

NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. CROSS SECTION.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

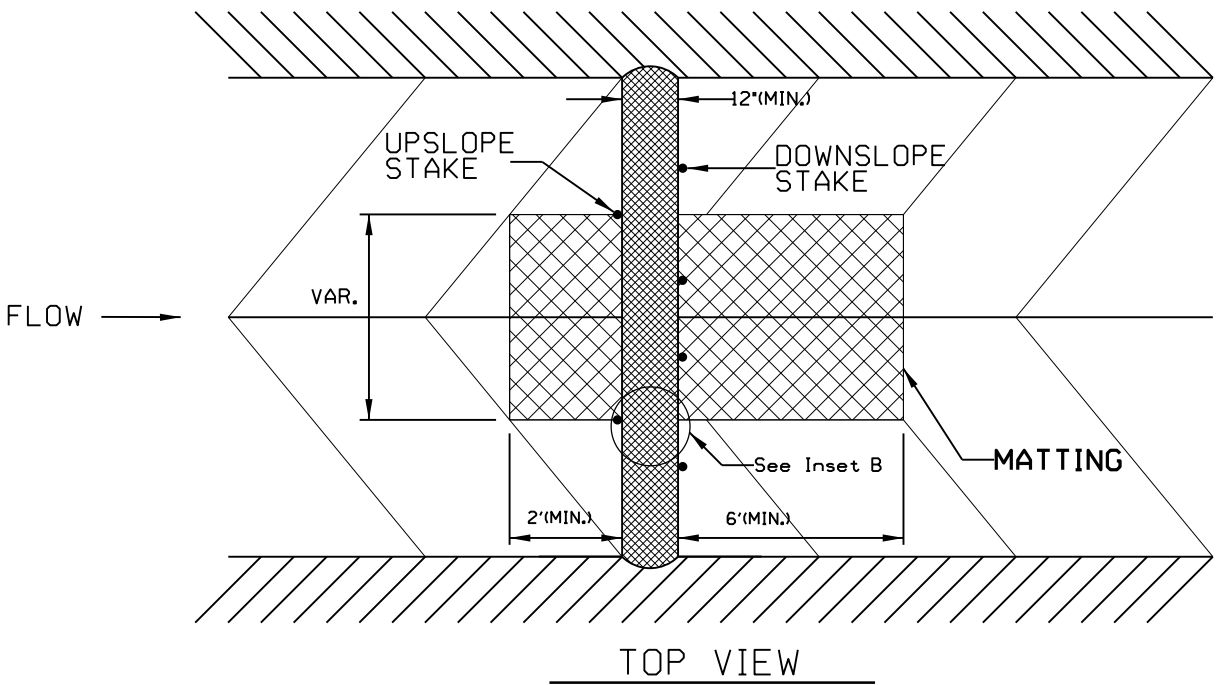
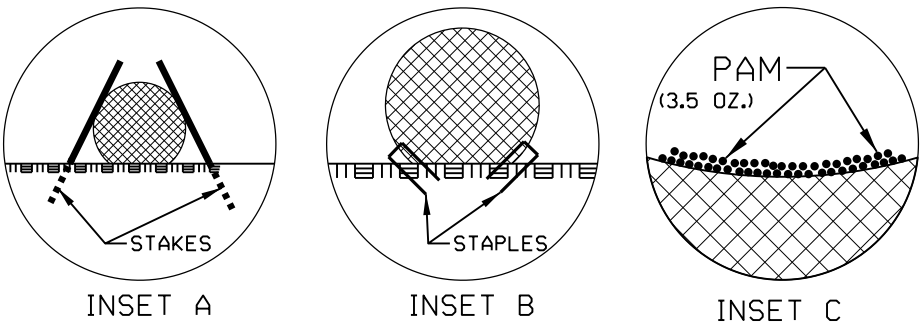
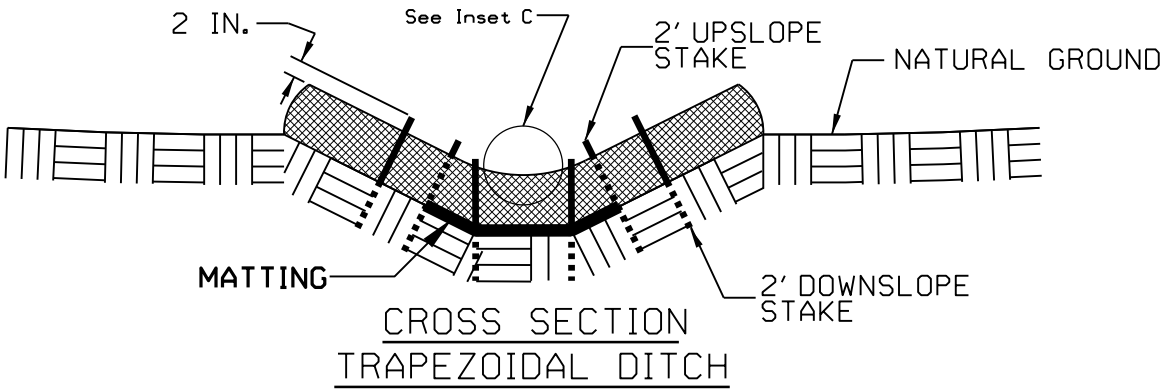
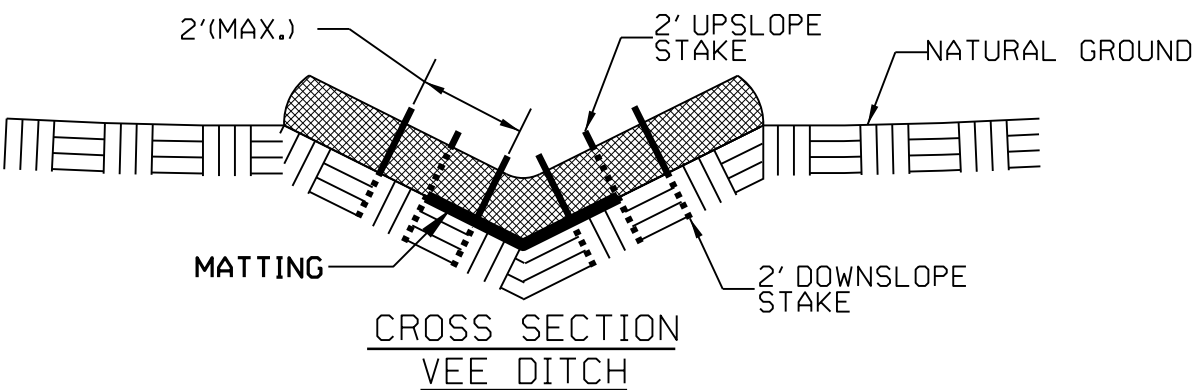
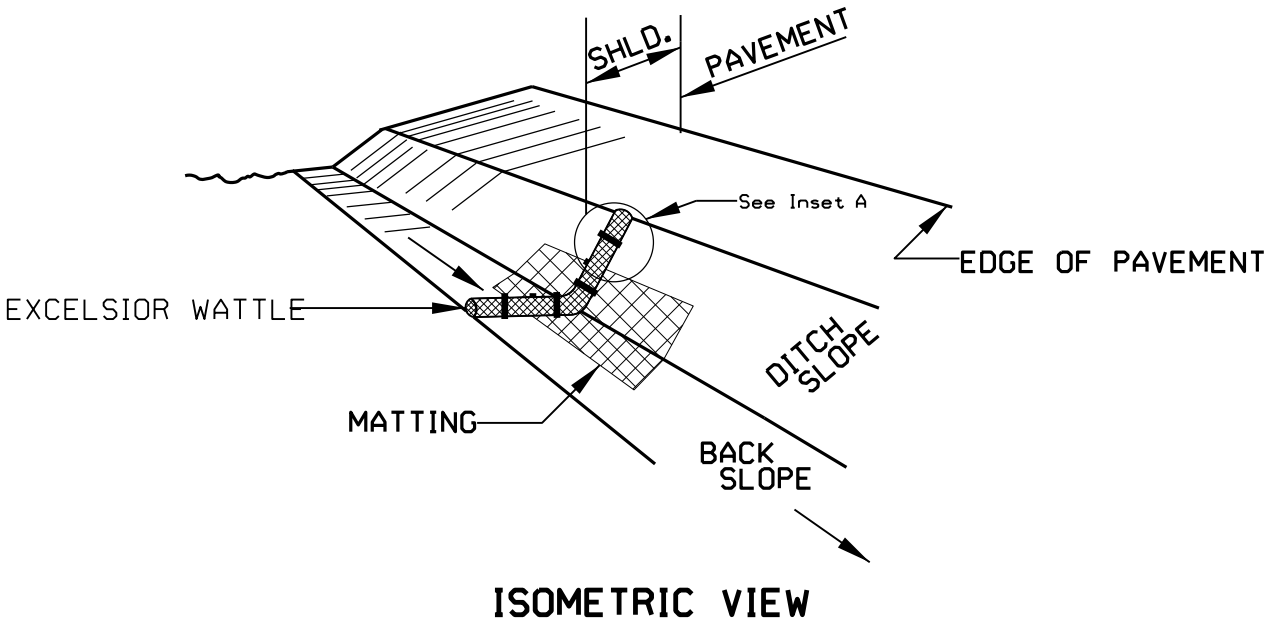
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

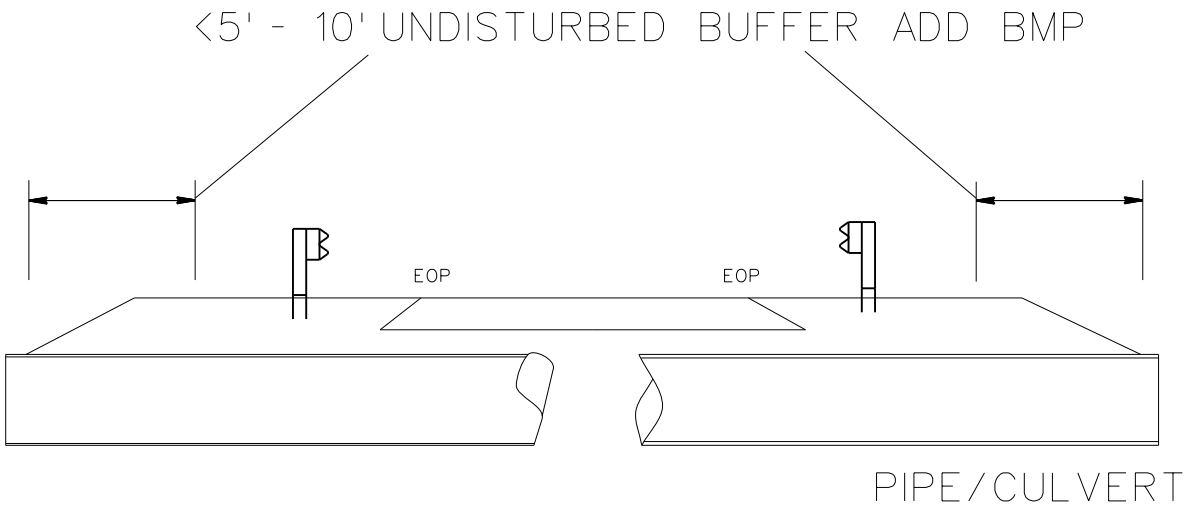
PRIOR TO POLYACRYLAMIDE (PAM) APPLICATION, OBTAIN A SOIL SAMPLE FROM PROJECT LOCATION, AND FROM OFFSITE MATERIAL, AND ANALYZE FOR APPROPRIATE PAM FLOCCULANT TO BE APPLIED TO EACH WATTLE.

INITIALLY APPLY 3.5 OUNCES OF ANIONIC OR NEUTRALLY CHARGED POLYACRYLAMIDE (PAM) OVER WATTLE WHERE WATER WILL FLOW AND AFTER EVERY RAINFALL EVENT THAT IS EQUAL TO OR EXCEEDS 0.50 IN.



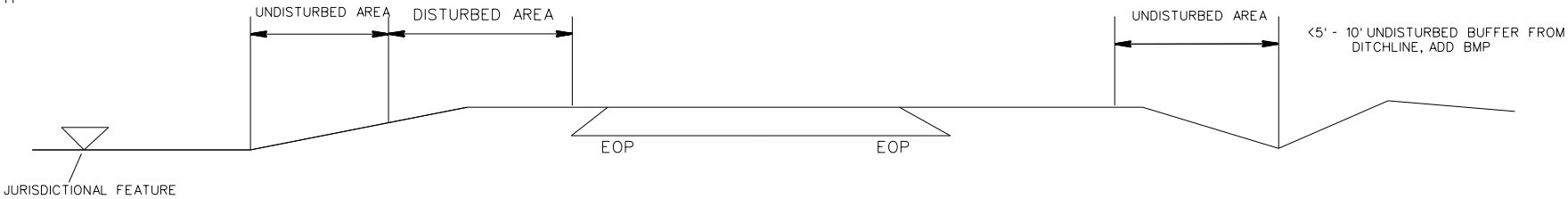
NOTES: LESS THAN 5' - 10' UNDISTURBED BUFFER FROM ROW, DITCHLINE, WATER FEATURE, OR DRAINAGE INLET, ADD BMP.

BMP OPTIONS: WATTLE OR SILT FENCE



STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2022CPT.10.11.10901 2022CPT.10.11.20901	EC2	
F.A. PROJECT NO.			

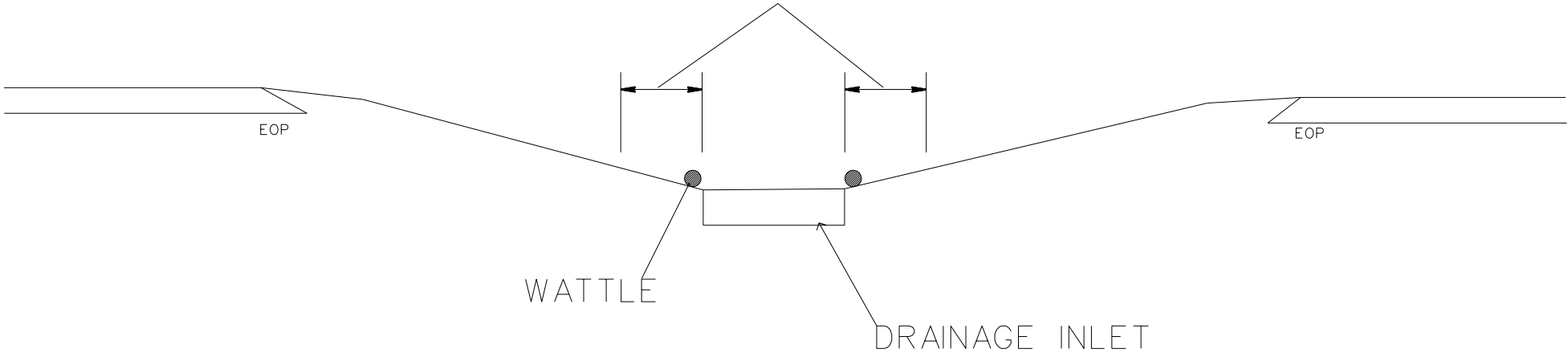
<5' - 10' UNDISTURBED BUFFER FROM JURISDICTIONAL FEATURE
ADD BMP




USE BMP'S IF SHOULDERS AND/OR FRONTSLOPES AND/OR DITCHLINE AND/OR BACKSLOPES ARE DISTURBED



<5' - 10' UNDISTURBED BUFFER FROM INLET, ADD WATTLE



EROSION CONTROL DETAIL			
SCALE	-NA-		REVISIONS
DATE	11/20		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED			

PROJECT NO.	SHEET NO.	TOTAL NO.
2022CPT.10.11.10901, 2022CPT.10.11.20901	24	24

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4510000000-N	4685000000-E		4695000000-E	4700000000-E	4720000000-E		4725000000-E			4810000000-E		4845000000-N			4891000000-E	4895000000-N
								MI	FT	SF	LS	HR	LF	LF	LF	LF	EA	EA	EA	EA	EA	LF	LF	EA	EA	EA	LF	EA
2022CPT.10.11.10901	Union	1	US 74	FROM PAVEMENT JOINT BEGINNING OF THE FIVE LANE SECTION IN WINGATE TO PAVEMENT JOINT AT THE END OF THE FIVE LANE SECTION IN WINGATE MILEPOST 16.24 TO 18.06	1,2	5	MU	1.82	68-86	126.0	1.00	40	21,000	8,495	1,090	585			52	8	8	21,000	8,495	52	8	8	500	482
TOTAL FOR MAP NO. 1								1.82		126.0	1	40	21,000	8,495	1,090	585			52	8	8	21,000	8,495	52	8	8	500	482
TOTAL FOR PROJ NO. 2022CPT.10.11.10901								1.82		126.0	1	40	21,000	8,495	1,090	585			52	8	8	21,000	8,495	52	8	8	500	482
													29,495						68			29,495		68				
2022CPT.10.11.20901	Union	2	SR 1758 AUSTIN CHANEY ROAD	FROM SR 1751 MONROE-ANSONVILLE ROAD TO PAVEMENT JOINT AT TRAFFIC CIRCLE MILEPOST 1.95 TO 2.24	3	2	2WU	0.29	22-23		*		3,016	3,016			5	8									24	
TOTAL FOR MAP NO. 2								0.29					3,016	3,016			5	8									24	
2022CPT.10.11.20901	Union	3	SR 1005 LANDSFORD ROAD	FROM PAVEMENT JOINT AT SR 1929 PHILIDELPHIA CHURCH ROAD TO SR 1934 CAMDEN ROAD MILEPOST 11.11 TO 9.94	5,6	2	2WU	1.17	21		*											19,444	24,640				24	
TOTAL FOR MAP NO. 3								1.17														19,444	24,640				24	
2022CPT.10.11.20901	Union	4	SR 2254 LANDSFORD ROAD	FROM SR 2114 MEDLIN ROAD TO SR 2115 STACK ROAD MILEPOST 4.5 TO 3.69	7	2	2WU	0.81	21.5		*											15,520	17,020				90	
TOTAL FOR MAP NO. 4								0.81														15,520	17,020				90	
2022CPT.10.11.20901	Union	5	SR 1934 CAMDEN ROAD	FROM SR 1937 OLD PAGELAND-MARSHVILLE ROAD TO SR 1935 TANNER ROAD MILEPOST 4.03 TO 5.43	8	2	2WU	1.4	18-19													26,048	29,160					
TOTAL FOR MAP NO. 5								1.4														26,048	29,160					
2022CPT.10.11.20901	Union	6	SR 1627 NEW SALEM ROAD	FROM PAVEMENT JOINT AT NC 205 TO SR 1659 LITTLE STATON ROAD MILEPOST 9.25 TO 8.3	3	2	2WU	0.95	23		*		7,900	10,050				4										
TOTAL FOR MAP NO. 6								0.95					7,900	10,050				4										
2022CPT.10.11.20901	Union	7	SR 1606 SIKES MILL ROAD	FROM UNION COUNTY LINE TO NC 218 MILEPOST 0 TO 2.86	3,4	2	2WU	2.86	19-22	320.3	*						5	16				55,497	60,480					
TOTAL FOR MAP NO. 7								2.86		320							5	16				55,497	60,480					
2022CPT.10.11.20901	Union	8	SR 2288 AUTUMN DRIVE	FROM SR 2115 STACK ROAD TO SR 2219 LAKE MONROE DRIVE MILEPOST 0 TO 0.13	9	2	2WU	0.13	18		*																	
TOTAL FOR MAP NO. 8								0.13																				
2022CPT.10.11.20901	Union	9	SR 2219 LAKE MONROE DRIVE	FROM END OF MAINTANANCE TO END OF MAINTANANCE MILEPOST 0 TO 0.47	9	2	2WU	0.47	18-19.5		*																	
TOTAL FOR MAP NO. 9								0.47																				
2022CPT.10.11.20901	Union	10	SR 2220 WOOD DUCK POINT	FROM SR 2219 LAKE MONROE DRIVE TO END OF MAINTANANCE MILEPOST 0 TO 0.13	9	2	2WU	0.13	18-19		*																	
TOTAL FOR MAP NO. 10								0.13																				
TOTAL FOR PROJ NO. 2022CPT.10.11.20901								8.21		320			10,916	13,066			10	28				116,509	131,300				138	
													23,982				38					247,809						
GRAND TOTAL								10.03		446.3	1	40	31,916	21,561	1,090	585	10	28	52	8	8	137,509	139,795	52	8	8	638	482
													53,477				38			68			277,304		68			